

# Village of La Grange

## AGENDA

PLAN COMMISSION  
of the  
VILLAGE OF LA GRANGE

Village Hall Auditorium

53 S. La Grange Road, La Grange, IL

**Tuesday, August 12, 2014 - 7:30 p.m.**



1. Call to Order and Roll Call of the Plan Commission
2. Approval of Minutes – July 22, 2014
3. Business at Hand:

**CONTINUED: PLAN COMMISSION CASE #219 – To consider a small planned development, special use permit and final site plans at 40 S. Ashland Avenue, 40 Ashland LLC and Michael Streit (Continued).**

4. Old Business:
5. New Business:
6. Adjournment

(Commissioners: Please call 579-2320 to confirm your attendance.)

Individuals with disabilities and who require certain accommodations to participate at this meeting are requested to contact the ADA Coordinator at 579-2315, to allow the Village to make reasonable accommodations.

## MINUTES

Plan Commission of the  
Village of La Grange  
July 22, 2014

### I. CALL TO ORDER AND ROLL CALL:

Chairman Kardatzke called the meeting to order on July 22, 2014, at 7:30 p.m. in the Village Hall Auditorium, 53 S. La Grange Road, La Grange, IL.

Present: Commissioners Reich, Egan, Weyrauch, Paice, and Stewart and Chairman Kardatzke.

Absent: Commissioner Williams.

Also present: Trustee Liaisons Jim Palermo and Bill Holder, Village Trustee David McCarty, Village Clerk John Burns, Assistant Community Development Director Angela Mesaros, Community Development Intern Katie Peterson, and Village Attorney Mark Burkland.

### II. APPROVAL OF MINUTES:

The Minutes of the July 08, 2014, Plan Commission meeting were presented for approval. It was moved by Commissioner Reich, seconded by Commissioner Weyrauch, that the Minutes be approved. Motion carried unanimously by voice vote.

### III. BUSINESS AT HAND:

**PLAN COMMISSION CASE #216– To consider approval of a planned development concept and final plan, an amendment to the La Grange Zoning map, a special use for a drive through facility, site plans, and design plans to authorize a mixed retail, multiple family development within the C-3 District at 31 East Ogden Avenue, Opus, Inc. (Continued)**

Chairman Kardatzke initiated the discussion by swearing in Sean Spellman, Vice President Opus Development and La Grange resident, and Dean Newman, Senior Vice President Architect Opus, Inc. Mr. Newins presented the revisions and answers to the questions from the June 10<sup>th</sup> meeting. The revisions included aspects of green space, retail circulation, Shawmut Ave., architecture, FAR justification, construction staging and compensating amenities.

The Commissioners and audience asked questions regarding traffic congestion, target renters, length of construction, building material and the necessity of retail at the north end

of the site. Comments were made by Commissioners and the audience expressing the improvements seen between the original concept and the design presented at this meeting. Audience members including residents of Beacon Place and La Grange Towers at 141 N. La Grange Road, also commented on the continued concern with traffic in the area if the plan moves forward.

There being no further questions or comments from the audience or the Commissioners, a motion was made by Commissioner Stewart, seconded by Commissioner Reich that the Plan Commission recommend to the Village Board of Trustees Approval of a planned development concept and final plan, an amendment to the La Grange Zoning map, a special use for a drive through facility, site plans, and design plans to authorize a mixed retail, multiple family development at 31 East Ogden Avenue.

Motion to APPROVE Carried by a roll call vote (6/0/1):

AYE	Reich, Egan, Paice, Weyrauch, Stewart, and Chairman Kardatzke.
NAY:	None.
ABSENT:	Williams.

**IV. OLD BUSINESS:**

None.

**V. NEW BUSINESS:**

None.

**VI. ADJOURNMENT:**

There being no further questions or comments from the audience or Commissioners, a motion to adjourn was made by Commissioner Reich, seconded by Commissioner Stewart. The meeting was recessed at 9:03 p.m.

Respectfully Submitted:

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Katie Peterson, Community Development Intern

VILLAGE OF LA GRANGE

Community Development Department

**MEMORANDUM**

TO: Plan Commissioners

FROM: Patrick D. Benjamin, Community Development Director  
Angela M. Mesaros, Assistant Community Development Director

DATE: August 12, 2014

**RE: CONTINUATION OF PLAN COMMISSION CASE #219 - (1) AMENDMENT TO ZONING MAP FROM C-1 TO R-8 MULTIPLE FAMILY RESIDENTIAL (2) PLANNED DEVELOPMENT CONCEPT/ FINAL PLAN APPROVAL FOR A MULTIPLE FAMILY RESIDENTIAL BUILDING, 40 South Ashland Ave., Michael Streit and 40 Ashland, LLC.**

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At the Plan Commission public hearing on July 8, 2014, the applicants, Michael Streit and 40 Ashland, LLC, presented applications to construct a multiple family development at 40 S. Ashland. (Staff prepared a report and recommendations for the July Plan Commission meeting. If you do not have a copy of the Staff Report, copies are available at the Community Development Department.)

Commissioners and Audience members had several questions and comments. Since the last meeting, the applicants have had discussions with Community Development Staff, Assistant Village Manager, Public Works Director and the Village's consulting engineer, Baxter & Woodman. In response, they have amended the proposal and provided additional information on the following items:

- **Concerns with flooding and capacity of the existing sewer system** – One of the key concerns from area residents is the impact of the development on drainage and public infrastructure. (Village-wide drainage and sewer capacity was discussed at a recent Village Board meeting, July 14, 2014, and is available on the Village's website.) Attached is a Memorandum from Director of Public Works, dated August 7, 2014, which outlines discussions with the applicant regarding impacts on the public infrastructure. Also attached is Baxter & Woodman's (Village's consulting engineer) review of the plans with the applicants' response to each item in green.
- **Traffic** – In 2005, Staff contracted with Metro Transportation to conduct a Traffic Study for a previous 12-unit mixed use development proposal. The applicant has provided an update to this study based on the current proposal for a 23-unit development. (See Attached Brand & Co., "Traffic Report.")

- **Capacity of the existing electrical system** – Residents expressed concerns about utility reliability as this neighborhood has experienced problems with the electrical system. After meeting with the Assistant Village Manager, the applicants have contacted ComEd to perform a load analysis to make sure that the site meets all requirements.
- **Density** – The applicants have reduced the number of residential units from 24 units to 23 units by combining the first floor units into one larger unit.
- **Parking** – Due to the reduction in the number of units by one, the development is now in compliance with the required number of parking spaces (23 units x 1.5 = 35 spaces).
- **Height** – Commissioners discussed the potential of reducing the building height by one story. According to the applicants, this would not be feasible. However, they have revised the building plans to slightly reduce height from 56' to 53' 8". (Note: As the required front yard is calculated based on height this requirement would be slightly reduced as indicated in the table below.)
- **Design of the Building** – According to the applicants, the buildings will be high quality design, finishes and landscaping. The applicant has made only minor revisions to the design.
- **Building Materials** – Changes to design include the addition of cement paneling and aluminum coping as well as the removal of the fiberglass cornice at the top of the building.
- **Impact on properties to the west** – Audience members expressed concerns about the height and bulk of the development, and the impacts on the "130 year old" houses directly to the west. According to the applicants, the proposed building is lower than the AT&T building and comparable in scale to other surrounding developments. This development would serve as a transition between the core retail area and the residential neighborhood.
- **Pervious pavers** – Commissioners commented that in order to reduce lot coverage, the applicants could extend the plaza pavers to the north and south section of the driveway apron. The applicants have agreed to this. Staff recommends a condition that these be properly maintained.
- **Rear Driveway** – The Village's consulting engineer had suggested that the north driveway (off of Ashland) be straightened to align with the alley across the street. After discussions, it was agreed to leave the drive as originally proposed.
- Other topics discussed at the hearing included access through the garages onto Ashland, snow and ice buildup on the buildings, construction nuisances, fiscal impact on the schools, consistency of zoning, and impact on line-of-site for Cossitt School's area cellular network.

With the revisions to the plans, including reduced building height and number of units, the project will require relief by Planned Development from the following areas:

1. Building Height (Number of stories)
2. Lot Area per Unit
3. Required Yards (Front, Corner Side, and Rear Yards)
4. Building Coverage
5. Lot Coverage

The specific amount of relief is noted in the following table:

<i>Standard</i>	<i>Required</i>	<i>Originally Proposed</i>	<i>Revised Application</i>
<b><i>Building Height</i></b>	3 stories, maximum 45 ft. With PUD, may be increased up to 5 stories or 70 ft.	5 stories Height: 56 ft.	Reduced to 53' 8"
<b><i>Lot Area per Unit</i></b>	Min. 1,300 sq. ft. per unit (Maximum 19 units)	24 units (1,055 sq. ft. per unit)	Reduced to 23 units (1,101 sq. ft. per unit)
<b><i>Front Yard</i></b>	Minimum 60% of building height or 25 ft. (whichever is greater)	Ashland Ave: 8.69 ft. <b>Required: Min 34 ft.</b> (56 ft. x 0.60 = 33.60)	With reduction in height, Required 32 feet (53.67 x 0.60 = 32.2)
<b><i>Corner Side Yard</i></b>	Minimum 17 ft.	Harris Ave: 11.5 ft.	No change
<b><i>Rear Yard</i></b>	Min. 20% of lot depth or 20 ft. (whichever is greater) <b>Required: Min 26 ft.</b> (130.45 X 0.20 = 26.09 ft.)	West property line: 15ft	No change
<b><i>Maximum Building Coverage</i></b>	Maximum 40% <b>Permitted: 10,126 ft<sup>2</sup></b>	15,401 ft. <sup>2</sup> (60.8%)	No change

<i>Standard</i>	<i>Required</i>	<i>Originally Proposed</i>	<i>Revised Application</i>
<b>Maximum Total Lot Coverage</b>	Maximum 60% <b>Permitted: 15,194 ft.<sup>2</sup></b>	20,230 square feet (79.9%)	No change
<b>Parking Spaces</b>	Multiple Family Residential: 1.5 spaces per dwelling unit <b>Required: min. 36 spaces</b>  (24 units x 1.5 =36)	35 spaces	Reduction to 23 units = 35 spaces required.  <b>Now In compliance with Zoning Code.</b>

At the hearing, the Applicants will present additional information and the public will have an opportunity to comment on the application. If the Plan Commission finds that the standards have been adequately addressed for the relief being sought, Staff recommends that the following action items be voted upon as separate motions by the Plan Commission. We also have prepared several conditions for your consideration as part of the development concept/final site plan approval. Additional conditions may also be desired by the Commission.

- 1. Amendment to the Zoning Map from C-1 Central Commercial to R-8 Multiple Family Residential;**
- 2. Revised Site Plans, dated August 6, 2014; and**
- 3. Special Use Permit/ Planned Development (including development concept plan and final plan) as submitted in Plan Commission Case #219, with the following conditions:**
  1. Lighting Plans. Location, size and arrangement of all exterior lighting be submitted by the applicant for compliance with the Code prior to issuance of a building permit, including photometrics and all fixtures.
  2. As part of the public contribution requirement to obtain relief under a Planned Development, the Applicant provide the following:
    - Monetary contribution (amount to be negotiated with Village staff prior to submission to the Village Board) to contribute to construction of (1) a section of water main on Harris to ensure sufficient quantity and security of water flow to the site, and (2) lining or replacement of the existing combined sewer on Ashland from Harris to the alley north of Harris.
  3. Sidewalks. Reconstruction of existing sidewalks to provide ADA-compliant routes.

4. Construction Staging Plan. Prior to issuance of a building permit the applicant shall prepare and file with the Village, for review and approval, a construction staging plan including delivery routes, construction parking, and street clean-up.
5. Grading and Stormwater Management plans shall be submitted and approved by the Village Engineer prior to Village Board approval.
6. Final Engineering Plans shall be approved by the Village prior to the issuance of any building permits.
7. Underground Utilities. Utility burial plan shall be approved by the Village prior to issuance of any building permits, and the applicant shall bury all on-site utility lines underground.
8. Landscaping Plans. Final landscaping details with a tree preservation plan shall be submitted with the application for building permit approval.
9. Materials. Final building material samples shall be identified fully as to manufacturer, make and model prior to Village Board approval. No deviation from approved materials will be allowed.
10. Screening. Refuse containers must be fully enclosed by an opaque fence, wall or densely planted evergreens of a height to completely screen such containers.
11. Conditions Covenants and Restrictions must be provided to satisfaction of the Village Manager.
12. Permits. Permits will be needed for outside entities including but not limited to MWRD and IEPA.
13. Soil Borings.
14. Fire Department review of hydrant spacing, location and connections prior to issuance of a building permit.
15. Signage. Final approval of materials and a sign package will be required before permitting and installation.

VILLAGE OF LA GRANGE  
Department of Public Works

**MEMORANDUM**

TO: Robert J. Pilipiszyn, Village Manager  
FROM: Ryan Gillingham, Public Works Director  
DATE: August 7, 2014  
RE: 40 S. Ashland Development  
Brand & Company  
Plan Commission Development Review

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The purpose of this memorandum is to provide comments for Plan Commission consideration related to the proposed development at 40 S. Ashland. This property is located at the northwest corner of Ashland and Harris Avenue. The comments contained within this memorandum are focused on the relationship and impact of the development on the existing public infrastructure and review of the proposed private infrastructure.

As background, Village staff and the development team met on April 7, 2014 to review the preliminary site plans provided by the developer. At this time preliminary staff comments were provided to the developer regarding the project including a general overview of the storm water management requirements that would be required for the site. On May 20, 2014, Village staff again had a detailed conversation with the developer regarding site development requirements. Specifically staff indicated to the developer that storm water detention would be required for the site with a condition that a minimum 3" restrictor size would be required. At the request of the developer the Village added this development to the list of developments submitted to the Metropolitan Water Reclamation District for exclusion from the newly adopted Cook County Watershed Management Ordinance. While the development was added to this list, adequate provisions for storm water management are still required per the Village regulations.

The Village then hired the consulting engineering firm Baxter & Woodman to perform an engineering review of the documents submitted for the proposed development based on Village codes, polices, and best engineering and development practices. Their review dated July 2, 2014 is attached to this memorandum for your reference. Please note that a number of the comments provided within the engineering review would be addressed as part of the final engineering for the development, but are important to note for the developer at this time so that an understanding of the requirements that will guide the development are known.

Baxter & Woodman's comments were provided to the developer and were included in the Plan Commission staff memorandum for the Plan Commission meeting on July 8, 2014.

On July 30, 2014, Village staff and representatives of Baxter & Woodman met with the development team to discuss the engineering comments contained with the memorandum dated July 2, 2014. Unfortunately the development team had not reviewed Baxter & Woodman's memorandum or provided additional engineering information prior to the July 30 meeting. In order to complete the preliminary engineering typical for a high level assessment of engineering issues, a topographical survey of the site is required. This activity has not been completed by the developer. Therefore, the information provided to date by the developer is insufficient to determine if the proposed development meets Village regulations for storm water management.

Therefore we recommend that, as a condition of development approval, the following storm water drainage standards be required:

1. The maximum storm water release rate from the site shall be limited to either 0.3 cubic feet per second per acre or the actual release rate from a 3" diameter restrictor.
2. On-site storm water detention shall be provided for the 24-hour, 100-year storm event, based on Illinois State Water Survey Technical Bulletin 70 Northeastern Illinois rainfall. Runoff hydrograph calculations (e.g., TR20 or TR55) need to be used.
3. The minimum freeboard shall be 3" from the top of the high water elevation in the clogged restrictor condition to the lowest garage floor elevation.

In summary we recommend that the Plan Commission require the developer to incorporate the above comments and those provided by Baxter & Woodman as a condition of approval for the development.

8840 W. 192nd Street  
Mokena, IL 60448  
815.459.1260  
708.478.8710  
www.baxterwoodman.com  
info@baxterwoodman.com



# Memo

**To: Ryan Gillingham, P.E., Director of Public Works**

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**From: Steve Amann, P.E., CFM**

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**Date: July 2, 2014** **Project No.: 140566.80**

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**Subject: 40 South Ashland – Preliminary Design Review**

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The following comments summarize the larger engineering issues pertaining to the planned residential development at the northwest corner of Ashland Avenue and Harris Avenue. This review is based on the items included in the development application package dated June 5, 2014. Please note that this is not a comprehensive review, since the plans do not have sufficient detail at this point to fully evaluate all of the applicable requirements.

### ***Overall Submittal***

1. A comprehensive soils report, based on site borings and analysis, is needed to design buildings and pavements, and analyze groundwater impacts on below-grade construction of buildings and utilities.

**We will provide the above requested documents at final engineering review.**

2. Construction permits will be needed from the following outside agencies, at a minimum:  
a. Metropolitan Water Reclamation District of Greater Chicago; and  
b. IEPA Division of Water Pollution Control (for sanitary sewer construction).

**Understood. However, we received a waiver from the new MWRD requirements as of May 1<sup>st</sup>, 2014.**

3. A complete, current topographical survey of the site and surrounding areas will be needed to describe current conditions, particularly the direction of runoff from the site, and the location of on-site improvements such as storm sewers and utility service connections for the previous building. This will be needed prior to preparation of the final engineering plans and supporting documents.

**We will provide the above requested documents at final engineering review.**

4. Construction of a section of water main on Harris Avenue is needed in conjunction with this development to ensure sufficient quantity and security of water flow to the site.

**Please quantify the cost with Village Staff and then we will come to a cash agreement.**

5. Lining or replacement of the existing combined sewer on Ashland Avenue from Harris to the

alley north of Harris is needed in conjunction with this development to ensure that the existing sewer will remain sound during the proposed construction.

**Please quantify the cost with Village Staff and then we will come to a cash agreement.**

***Grading and Stormwater Management***

The detention facilities and release design for this site need to address the following issues:

6. Since this site has been vacant for eight years, the proposed runoff needs to be compared to the current conditions as well as the previously-developed conditions.

**Understood.**

7. The maximum release from this site in the 100-year event needs to be limited to 0.30 cubic feet per second per acre, or the existing release from this site, whichever is less. The design has to demonstrate a net benefit to the Village's already-overloaded combined sewer system.

**We commit to making a 100% improvement on current conditions, bringing the site from 880 CF to 1760 CF.**

8. Supporting calculations are needed for the design, including runoff hydrograph modeling, runoff curve number calculations, time of concentration calculations supported with flow path diagrams, and restrictor sizing calculations reflecting tailwater conditions in the receiving sewer. The MWRDGC's nomograph for small sites, if applicable per the MWRDGC, may be used to determine the required storage volume.

**We will provide the above requested documents at final engineering review. Conceptual calculations have been provided. Calculations including tailwater conditions will be provided after the topographic survey is completed.**

9. The term "CFS-AC" used in the narrative needs to be explained.

**It is a typo. It should say CFS (cubic feet per second)**

10. The existing on-site catch basin (and any other on-site or adjacent drainage facilities not currently discussed) needs to be included in the stormwater analysis and design.

**On site catch basin will be included after the topographic survey is completed.**

11. Assumptions regarding existing runoff patterns and runoff coefficients need to be replaced with supported calculations.

**Calculations will be provided after the topographic survey is completed.**

12. Rooftop detention is strongly discouraged because of the potential for this stormwater to leak into the building.

**Our design does not have rooftop detention over habitable living areas. Rooftop detention is isolated to areas over uninhabited private garages.**

13. If runoff will be stored in the parking lot, it will need to be provided with an overland flow route with a calculated high water level at least one foot below the lowest opening into the structure or any adjacent existing structures.

**Our design includes driveway storage. We will provide an overland route. Relation of the high water table to the adjacent existing structures will be provided after the topographic survey is completed. We will make every effort to provide 12" between the high water table and the finished**

**floor elevation of the proposed building; however the entrance to the private garage may be set 2" above the high water level.**

14. 100-year flow routes are needed for the entire site, including the building.

**We will provide the above requested documents at final engineering review.**

15. Downspouts need to be connected to the storm sewer system.

**Understood.**

16. The eight-inch outlet pipe needs to be at least a 12-inch pipe, with a restrictor (minimum 3-inch diameter) located inside an accessible, maintainable structure.

**Outlet storm pipe will be changed to 12". Restrictor will be sized to provide detention volume described in item no. 7**

*Utilities*

17. The existing utility services to this site need to be disconnected in accordance with Village requirements.

**Understood.**

18. Fire Department review is needed for the hydrant spacing and locations, as well as the Fire Department Connections on the building.

**Our design does not include installing hydrants or modifying existing hydrants. Our design does include a Siamese connection pursuant to NFP requirements.**

19. The final engineering and building submittals will need to include domestic and fire water service sizing calculations per the Illinois Plumbing Code, reflecting fire flow tests to be done by the developer.

**Understood.**

*Access and Circulation*

20. The rear driveway needs to be straightened out. Maneuvering the proposed reverse curve will be difficult and dangerous for passenger vehicles, and even more so for delivery, garbage and emergency vehicles. In addition, it should line up with the existing alley on the east side of Ashland Avenue, decreasing the number of conflict points on Ashland.

**Our design provides a safe vehicular entrance that optimizes pedestrian safety. A straightened driveway, as proposed in this memorandum, would put the driveway adjacent to the AT&T Building and require the front half of each vehicle to have crossed the sidewalk before the driver could react to pedestrians.**

21. The existing sidewalks on and around the site will need to be reconstructed to provide an ADA-compliant accessible route.

**Understood. However, this condition only applies to existing public walks on frontage of this site.**

22. The applicant needs to develop and estimate of the expected site-generated traffic, based on standard land use categories in the Institute of Transportation Engineer's Trip Generation Manual.

**We have estimated the expected site generated traffic based on Metro report.**

During the course of this review, we noted additional items which will need to be addressed prior to final plan approval. It would be premature to attempt to list, address and resolve these issues since the plans

will likely go through several revisions prior to becoming final construction documents. Additional information and calculations will be necessary to support the design.

Please let us know if you have any questions or need any additional information.

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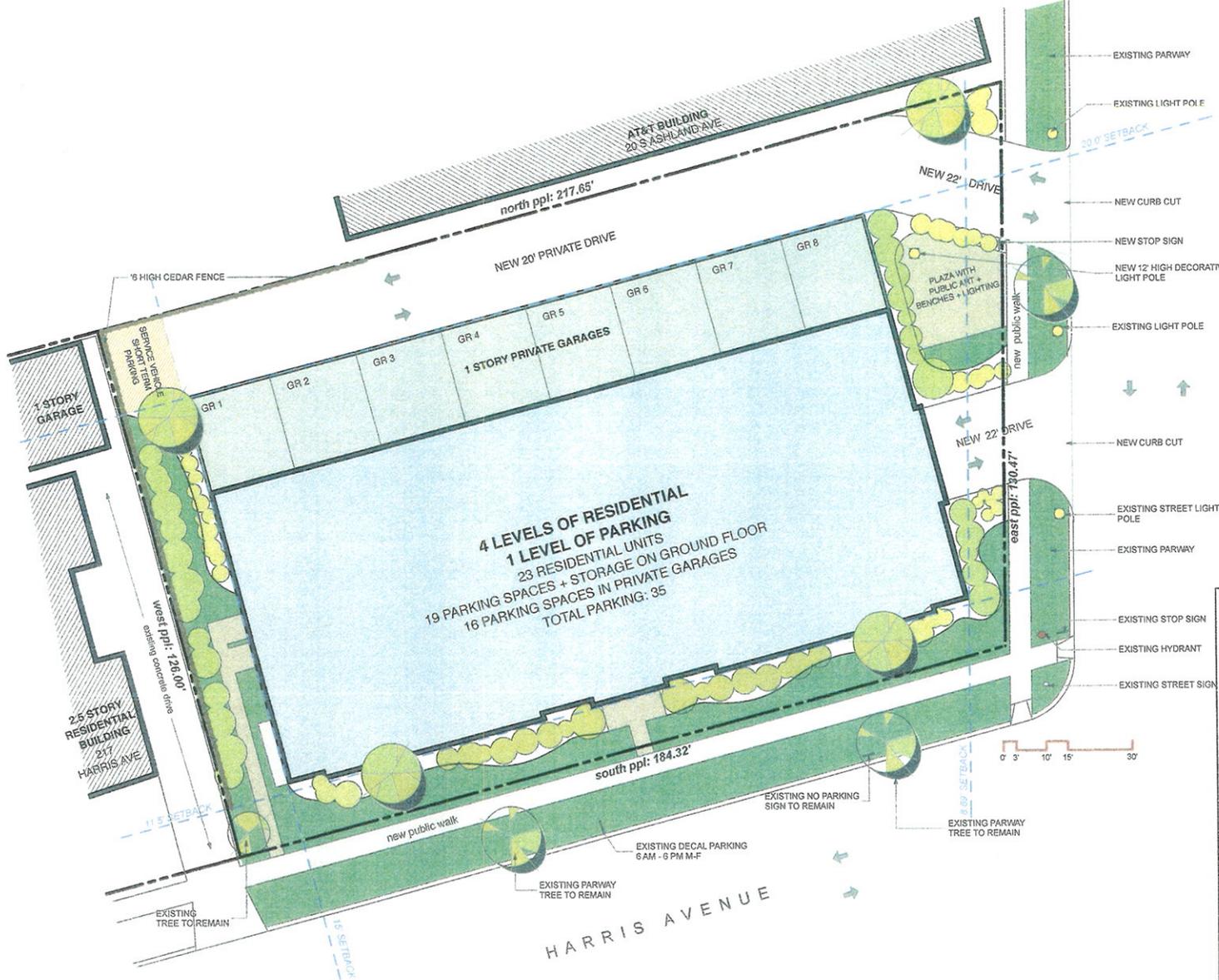
## PROJECT DATA

- Project Name **ASHLAND + HARRIS DEVELOPMENT** ■ Date: August 6, 2014
- Project Address **40 Ashland Avenue LaGrange, Illinois** ■ Prepared By: john conrad schiess architect ltd.
- Lot Dimensions **184.32' x 130.47' x 217.65' x 126.00' IRREGULAR**
- Lot Area **25,324.01 SF**

## ZONING ANALYSIS

ITEM	SECTION	REGULATIONS	ALLOWED/REQUIRED	PROPOSED	ALLOWANCE
Zone District	5-102	Central Commercial	<i>Commercial</i>	R-8 Multiple-Family Residential	re-zone to R-8
Allowable Height Max.	4-110	3 stories or 45'-0" whichever is less	<i>3 stories or 45'-0"</i>	5 stories (53'-8")	allow 8'-8" additional
Min. Lot Area	4-110	12,000 sq ft	<i>12,000 sq ft</i>	25,324.01 sq ft	none
Lot Area per Unit	4-110	1,300 sq ft	<i>19 unit</i>	23 unit	4 units
Setbacks Min.	4-110				
Front (east)		60% of building height	<i>32.0' required</i>	8.69'	23.31'
Rear (west)		20% lot depth	<i>26.09' required</i>	15.0'	11.09'
Side (south + north)		corner side yard: 17.0' (south) interior side yard: 13.0' (north)	<i>corner side 17.0' int. side yard 18.4'</i>	corner side 11.5' (south) interior side yard 20.0' (north)	5.5' (south) none (north)
Min. Dwelling Unit Size	4-110	1BR - 650 sq ft, 2BR - 850 sq ft, 3BR - 1,000 sq ft, 4 BR -1,150 sq ft	<i>2 BR - 850 sq ft 3 BR - 1,000 sq ft</i>	2 BR - 1,236 sq ft 3 BR - 2,830 sq ft	none
Max Bldg. Coverage	4-110	40%	<i>10,129.6 sq ft</i>	15,401.52 sq ft	5,271.92 sq ft
Max Total Lot Coverage	4-110	60%	<i>15,194.41 sq ft</i>	20,230.74 sq ft	5,036.34 sq ft
Parking					
Residential	10-101-102	1.5 for each dwelling unit	<i>35 required</i>	35	none

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<b>SITE DATA:</b>	
LOT DIMENSIONS :	184.32' x 130.47' x 217.65' x 126.0'
LOT AREA /	25,355 SQ FT
USABLE OPEN SPACE:	0.582 ACRES
<b>PROPOSED :</b>	
LOCATION:	40 SOUTH ASHLAND AVENUE
BUILDING SIZE:	164'-0" x 93'-10"
USE:	R-S MULTIPLE - FAMILY
BUILDING HEIGHT:	5 stories (55'-0")
FAR:	62,115.92 (gross) / 25,355 SQ FT = 2.44
BUILDING COVERAGE:	15,401.52 SQ FT (gross)
TOTAL IMPERVIOUS:	20,230.74 SQ FT
<b>PROPERTY INDEX NUMBERS:</b>	
	18-04-124-020-0000
	18-04-124-021-0000
	18-04-124-022-0000
	18-04-124-023-0000

**SITE PLAN**  
40 S. Ashland Avenue, LaGrange, IL 60525

Revised for PD	08/06/14
Issue for Site Plan	
Approval	05/28/14
Issue for Preliminary Review	05/08/14
	Date

The RESIDENCES  
at  
ASHLAND AVENUE  
40 S. Ashland Avenue  
La Grange, Illinois

Sheet Title  
site plan  
**SK1.1**  
Sheet No.



**FIRST FLOOR PLAN**

40 S. Ashland Avenue, LaGrange, IL 60525

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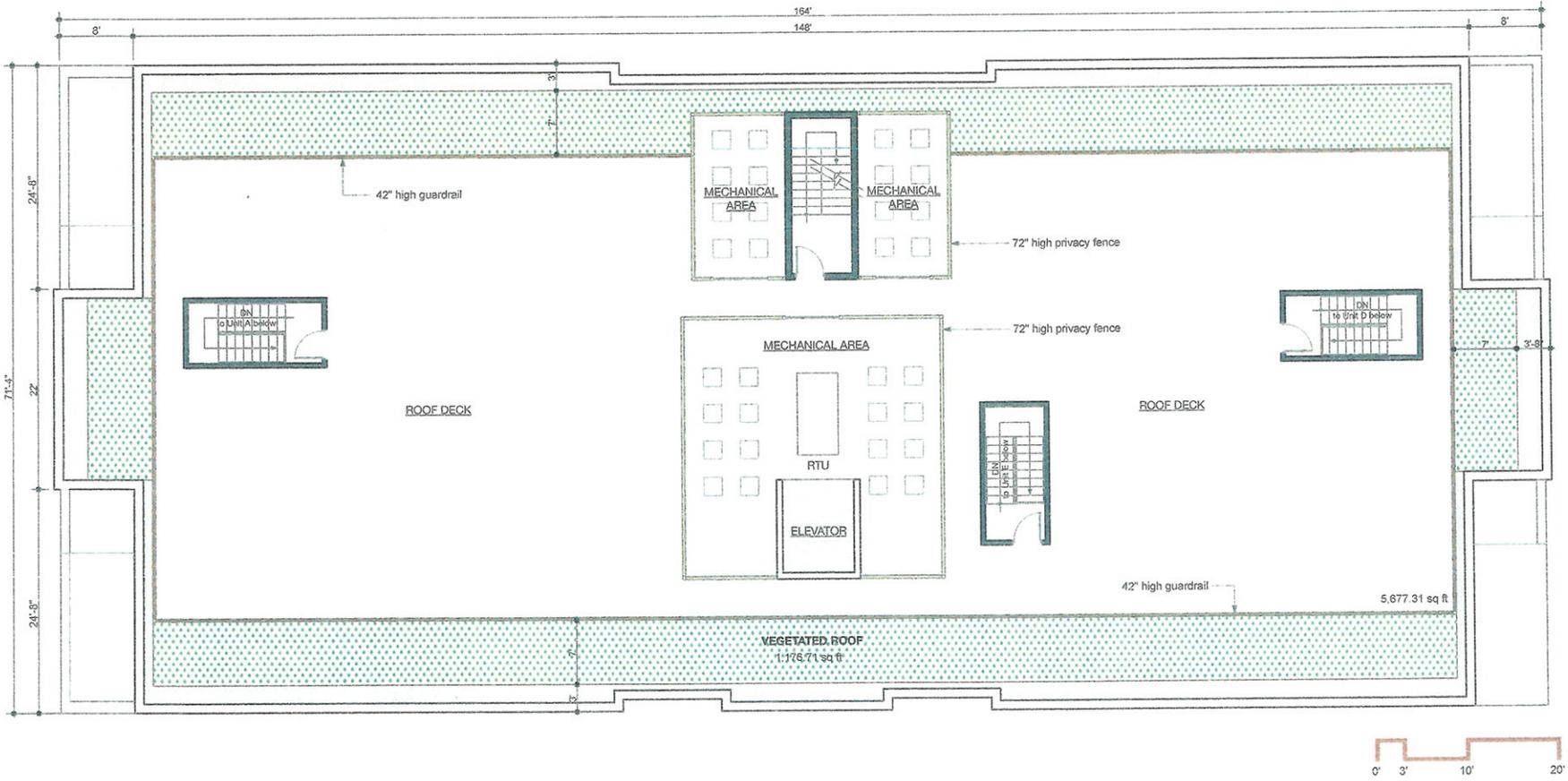
Revised for PD	08/06/14
Issue for Site Plan	05/28/14
Issue for Preliminary	05/08/14
Review	Date

The RESIDENCES  
 at  
 ASHLAND AVENUE  
 40 S. Ashland Avenue  
 La Grange, Illinois

Sheet Title  
 first floor

SK1.2

Sheet No.



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Revised for PD	08/06/14
Revised for Site Plan Approval	07/01/14
Issue for Site Plan Approval	05/28/14
Issue for Preliminary Review	05/08/14
Date	

The RESIDENCES  
at  
ASHLAND AVENUE  
40 S. Ashland Avenue  
La Grange, Illinois

Sheet Title  
roof plan

# SK1.7

Sheet No.



## ROOF PLAN

40 S. Ashland Avenue, LaGrange, IL 60525

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**FRONT ELEVATION**  
40 S. Ashland Avenue, LaGrange, IL 60525

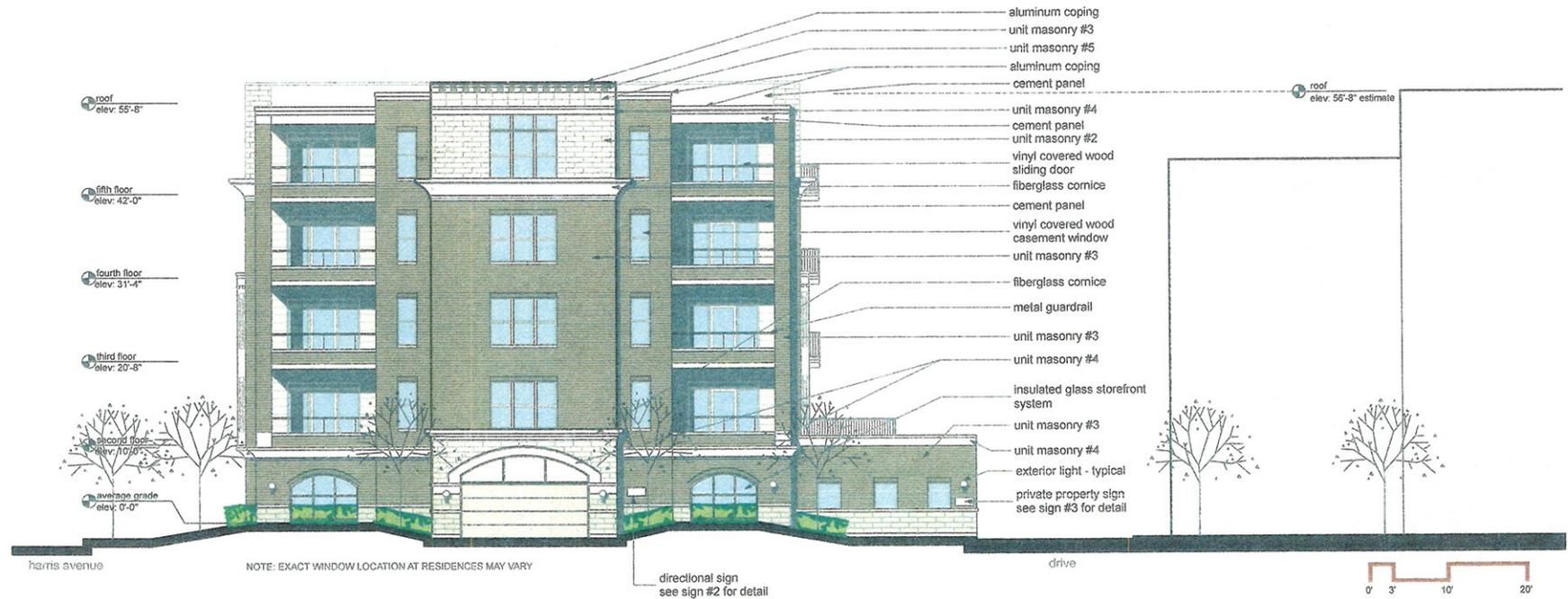
Revised for PD	08/06/14
Revised for Site Plan	
Approval	07/01/14
Issue for Site Plan	
Approval	05/28/14
Issue for Preliminary	
Review	05/08/14
	Date

The RESIDENCES  
at  
ASHLAND AVENUE  
40 S. Ashland Avenue  
La Grange, Illinois

Sheet Title  
elevation

**SK2.1**  
Sheet No.

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**EAST ELEVATION**

40 S. Ashland Avenue, LaGrange, IL 60525

Revised for PD	08/06/14
Revised for Site Plan Approval	07/01/14
Issue for Site Plan Approval	05/28/14
Issue for Preliminary Review	05/08/14
Date	

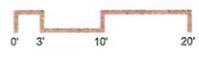
The RESIDENCES  
 at  
 ASHLAND AVENUE  
 40 S. Ashland Avenue  
 La Grange, Illinois

Sheet Title  
 elevation

SK2.2

Sheet No.

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**WEST ELEVATION**  
 40 S. Ashland Avenue, LaGrange, IL 60525

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Issue for Preliminary Review	05/08/14
Date	

The RESIDENCES  
 at  
 ASHLAND AVENUE  
 40 S. Ashland Avenue  
 La Grange, Illinois

Sheet Title  
 elevation

# SK2.3

Sheet No.

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- aluminum coping
- unit masonry #5
- unit masonry #3
- unit masonry #4
- cement panel
- aluminum coping
- unit masonry #2
- fiberglass cornice
- unit masonry #3
- vinyl covered wood casement window
- aluminum coping
- cement panel
- unit masonry #3
- unit masonry #3
- 72" high privacy fence (typ.)
- aluminum guardrail
- aluminum gutter + downspout
- exterior light - typical
- unit masonry #3
- overhead garage door
- unit masonry #1

Revised for PD	08/06/14
Revised for Site Plan	
Approval	07/01/14
Issue for Site Plan	
Approval	05/28/14
Issue for Preliminary	
Review	05/08/14
	Date

The RESIDENCES  
 at  
 ASHLAND AVENUE  
 40 S. Ashland Avenue  
 La Grange, Illinois

Sheet Title  
 elevation  
**SK2.4**  
 Sheet No.

**NORTH ELEVATION**

40 S. Ashland Avenue, LaGrange, IL 60525

NOTE: EXACT WINDOW LOCATION AT RESIDENCES MAY VARY



## *Traffic Report*

Ashland Avenue at  
Harris Avenue

LaGrange, IL

August 6, 2014

## Introduction

In 2005 Metro Transportation Group (Metro) was retained by the Village of La Grange to conduct a traffic impact study for a mixed-use development on the site located at the northwest corner of the intersection of Ashland Avenue and Harris Avenue. The report analyzed the traffic-related impact from a mixed use development that included 4,850 square foot of ground floor retail, two floors with a total of 12 residential units (18,920 sf) and a 24 space parking garage.

The data in the Metro report has been utilized to examine the impact of a proposed 23 unit residential development on the same site.

## Proposed Development

The proposed residential development of this site includes five floors with a total of 23 residential units and 35 parking spaces. Access to the development will be from two driveways located on Ashland Avenue. One drive will provide access to 19 enclosed parking spaces located on the ground floor of the building. The second drive will access 16 parking spaces located in eight private garages on the north side of the development. A site plan illustrating the driveway locations and the private garages is attached (Drawing SK1.1).

## Vehicle Trip Generation

The Metro study projected traffic volumes specific to the retail and residential aspects of the 2005 development. The volumes were calculated taking the average rate versus the equation rate from the Institute of Transportation Engineers (ITE) in Trip Generation, 7<sup>th</sup> Edition, as the average rates seemed "more realistic".

Metro estimated the Total New Trips generated by the residential portion of the 2005 project would be 4 new trips in the peak AM hour, and 5 new trips in the peak PM hour. As the proposed development includes 23 residential units versus 12 (with no retail) it is conservative to double the estimated Total New Trips projected for the residential development in the 2005 report. Table 1 below illustrates the projected Total Traffic Volume generated by the proposed development. It is estimated that the development will generate a total of 8 new bi-directional trips during the weekday morning peak hour, and 10 bi-directional trips during the afternoon peak hour.

Proposed Development 23 Residential Units	Weekday A.M Peak Hour			Weekday P.M. Peak Hour		
	In	Out	Total	In	Out	Total
<b>Total New Trips:</b>	<b>2</b>	<b>6</b>	<b>8</b>	<b>6</b>	<b>4</b>	<b>10</b>

Based on the summary of vehicle movements provided by Metro, 8 new trips during the peak AM hour will account for only 1.8% of all vehicle movements during that time period. 10 new trips in the peak PM hour will account for only 1.5% of all vehicle movements during that time period.

TIME	ASHLAND TOTAL	HARRIS TOTAL	TOTALS	DEVELOPMENT GENERATED TRAFFIC
7:00 - 8:00 AM	178	181	359	
7:15 - 8:15 AM	148	270	418	
7:30 - 8:30 AM	141	314	455	
7:45 - 8:45 AM	131	326	457	
<b>8:00 - 9:00 AM</b>	171	255	426	<b>(8) or 1.8%</b>
2:30 - 3:30 PM	241	285	526	
2:45 - 3:45 PM	230	298	528	
3:00 - 4:00 PM	285	273	558	
3:15 - 4:15 PM	333	245	578	
3:30 - 4:30 PM	364	204	568	
3:45 - 4:45 PM	403	195	598	
4:00 - 5:00 PM	414	190	604	
4:15 - 5:15 PM	428	158	586	
<b>4:30 - 5:30 PM</b>	437	196	633	<b>(10) or 1.5%</b>
4:45 - 5:45 PM	434	195	629	
5:00 - 6:00 PM	422	149	571	
5:15 - 6:15 PM	410	213	623	
5:30 - 6:30 PM	377	200	577	
5:45 - 6:45 PM	362	191	553	
6:00 - 7:00 PM	342	175	517	
<b>TOTALS</b>	<b>6,251</b>	<b>4,513</b>	<b>10,764</b>	

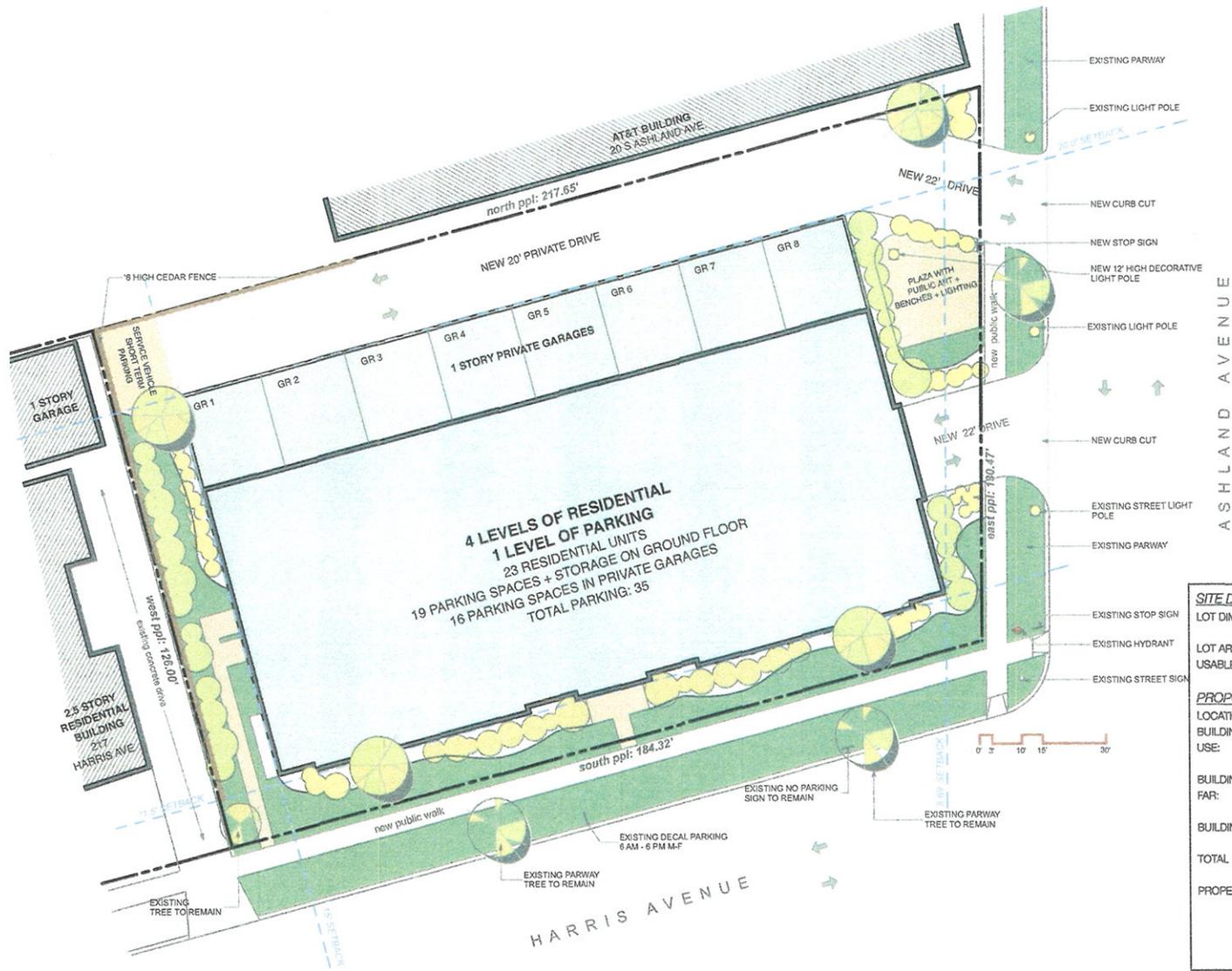
## Conclusion

In 2005 Metro concluded that based on a projection of 22 new trips per day during weekday peak morning and afternoon hours from the proposed mixed use development, "vehicle and pedestrian traffic generated by the development can be accommodated".

The proposed development reduces the estimated new volume to only 18 new trips during the peak morning and afternoon hours, adding less than 2% to all vehicle movements during the peak periods.

Based on this data and the previous Metro conclusions, development of the 40 South Ashland site with 23 residential units will not have a negative impact on current traffic in the Ashland and Harris Avenue neighborhood, and therefore the development can be accommodated.

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<b>SITE DATA:</b>	
LOT DIMENSIONS :	184.32' x 130.47' x 217.65' x 126.0'
LOT AREA /	25,355 SQ FT
USABLE OPEN SPACE:	0.582 ACRES
<b>PROPOSED :</b>	
LOCATION:	40 SOUTH ASHLAND AVENUE
BUILDING SIZE:	164'-0" x 93'-10"
USE:	R-8 MULTIPLE - FAMILY RESIDENTIAL
BUILDING HEIGHT:	5 stories (55'-0")
FAR:	62,115.92 (gross) / 25,855 SQ FT = 2.44
BUILDING COVERAGE:	15,401.52 SQ FT (gross)
TOTAL IMPERVIOUS:	20,230.74 SQ FT
<b>PROPERTY INDEX NUMBERS:</b>	
	18-04-124-020-0000
	18-04-124-021-0000
	18-04-124-022-0000
	18-04-124-023-0000

**SITE PLAN**  
40 S. Ashland Avenue, LaGrange, IL 60525

Revised for PD	08/06/14
Issue for Site Plan	
Approval	05/28/14
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	Date

The RESIDENCES  
at  
ASHLAND AVENUE  
40 S. Ashland Avenue  
La Grange, Illinois

Sheet Title  
site plan  
**SK1.1**  
Sheet No.